



Design Review Subcommittee (DRSC)

Meeting Date: January 23, 2019

PLANNER: Veronica Morones, Assistant Planner

SUBJECT: **Pre-Application Review 18-234, In-N-Out**, a request to demolish the existing two-story bank building and build a new drive-through restaurant with outdoor dining area. The project is located within the West Pico Corridor Specific Plan (WPCSP) in the Community Commercial area.

BACKGROUND:

The project site is located at 115 Via Pico Plaza, at the intersection of Avenida Pico and Via Pico Plaza, within the Pico Plaza shopping center. Pico Plaza is comprised of eight parcels and was developed under one site development plan in the late 1970s. The 47,787 square foot lot is currently developed with a 6,708 square foot two-story office building occupied by California Bank and Trust. The site is an irregularly shaped parcel, with the primary frontage located on Via Pico Plaza and a small portion of street frontage facing Avenida Pico. Figure 1a and 1b depict two of the site's street frontages. Attachment 2 provides additional site photographs. Surrounding land uses include commercial uses to the north, south, and west, as well as a USPS post office to the east.

The applicant is proposing to demolish the existing two-story office building and develop a 3,882 square foot drive-through restaurant with outdoor dining area. The City of San Clemente's General Plan, Urban Design Element, Figure UD-1 identifies the site as a freeway gateway, and therefore requires Spanish Colonial Revival (SCR) design.

The application was reviewed by the Development Management Team (DMT) on two separate rounds of review.

Figure 1a: Existing Conditions
(Avenida Pico Frontage)

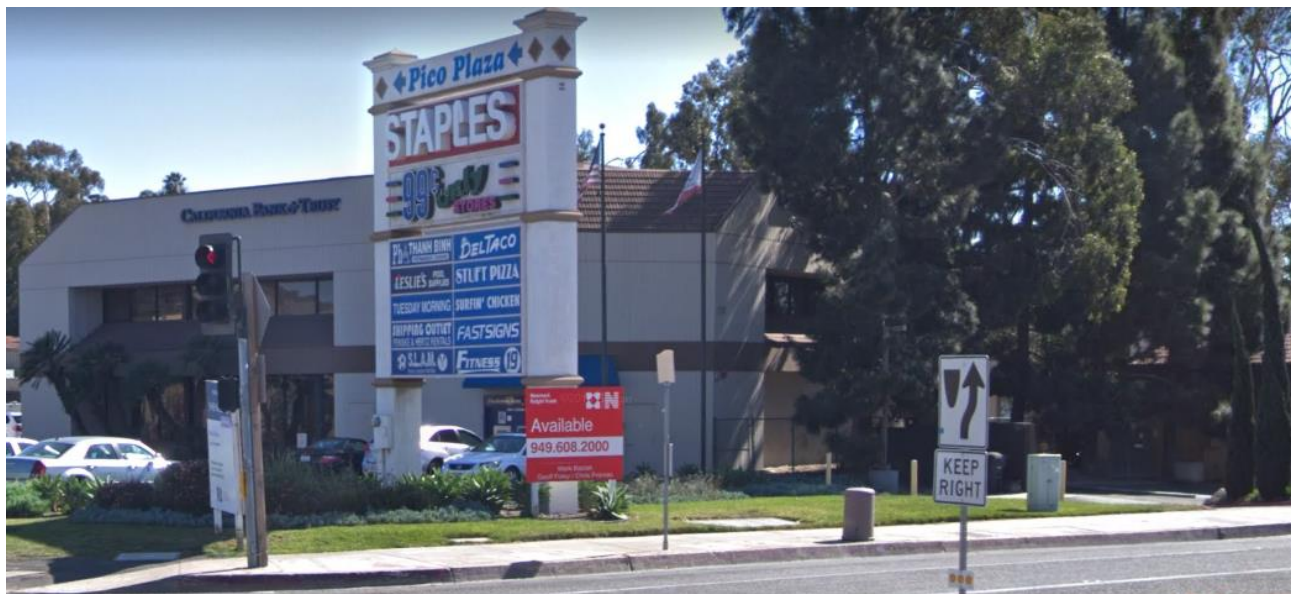


Figure 1b: Existing Conditions
(Via Pico Plaza Frontage)



Why is DRSC Review Required?

The intent of the pre-application process is allow the applicant to become more familiar with the City's review process and to identify issues relative to the project. DRSC review is requested for comments concerning the site design and building architecture.

ANALYSIS:

Per Zoning Ordinance Section 17.12.030, Pre-Application Review, the following are the primary purposes of pre-application review of a development project:

- To familiarize the applicant with the application requirements and the review process for a project;
- To familiarize the applicant with City documents affecting a project, including, but not limited to, the General Plan, Design Guidelines and Zoning Ordinance;
- To ensure early consideration of General Plan goals and policies;
- To identify for the applicant some of the significant issues and community concerns that may arise as the project moves through the review process;
- To provide Planning Division staff with the opportunity, when appropriate, to bring in staff from other divisions and departments for early consultation on a project;
- To determine the appropriate reports and studies that will be necessary for the review of the project, including, but not limited to, traffic reports, noise studies and fiscal impact analyses.

Site Design

The applicant proposes to relocate the existing entry driveway, shifting it approximately 13-feet to the south. This driveway is the only entrance to the parcel that is located within the legal property bounds of the subject lot. A second driveway is located on the adjacent parcel to the south, with reciprocal access to the project site granted by a recorded agreement. The drive-through lane entrance would be located at the rear of the lot, and the exit would be located near the front entry driveway. Access onto the site via the first driveway requires vehicles accessing the drive-through to drive to the back of the lot, which results in vehicles passing the drive-through exit before entering the drive-through lane. The secondary entrance is located on the adjacent lot, and would require vehicles to drive through the adjacent office building's parking lot. Traffic engineering and planning staff note the proposed site plan may cause circulation issues if the exit lane becomes blocked by on-site

vehicles. Forty-five parking spaces and a trash enclosure are provided in the center of the parcel, partially shielded behind the building. An aggregate parking analysis of all uses located within Pico Plaza is required for determining the total parking demand for the restaurant use, and was not provided by the applicant under this review. A detailed analysis will be provided to the Subcommittee in future reviews of the project.

Avenida Pico is a General Plan Major Urban Corridor with policies to aid in the view preservation of scenic corridors throughout San Clemente. As such, the applicant proposes the restaurant along the Avenida Pico frontage, with the drive-through, parking lot, and trash enclosure predominantly located in the rear. A 64-foot setback from Avenida Pico, in conjunction with the placement of the outdoor dining area, provides a pedestrian-oriented buffer between Avenida Pico and the restaurant, and acts as a visual buffer to the top corner of the drive-through lane.

Architecture

The applicant focused a majority of the pre-application review on site design, with significant revisions made to the proposed site plan between the first and second DMT reviews. The project utilizes an architectural design typical of In-N-Out restaurants. The proposed structure is consistent with some basic elements of SCR, such as smooth white stucco and red tile roofing. However, the structure is inconsistent with the City's Design Guidelines and WPCSP overall. The building is vertical in orientation and utilizes a large amount of parapet wall for the roof line. Red L.E.D. light strips and awnings are proposed all the way around the building inconsistent with the General Plan's Dark Skies policies, City Design Guidelines, and WPCSP.

Landscaping and Signage

At this time, there is little information provided on the proposed landscaping and signage. A detailed analysis will be provided to the Subcommittee in future reviews of the project.

There are three street trees located on the Via Pico Plaza frontage, and no trees located on the Avenida Pico frontage. Conceptual landscape plans will be required if a formal application is submitted.

Current signage on-site consists of a 25-foot monument sign for Pico Plaza. The plans identify a proposed monument sign for In-N-Out on the Via Pico Plaza frontage near the entry driveway to the parcel, and a change in copy on the 25-foot monument sign to include In-N-Out. Additional on-site directional signs, menu board, loud speaker, as well as building signs are proposed. A Discretionary Sign Permit will be required based on the number and type of signs shown on plans.

RECOMMENDATIONS:

Staff provided preliminary comments regarding elements of SCR style that could be improved upon during the first pre-application review submittal. The following are

recommendations to help the development be more consistent with the Design Guidelines, General Plan and the WPCSP. Additionally, Attachment 3 provides visual examples of staff recommendations.

Site Design

1. The use of on-site circulation mitigation, such as keep clear signage, roadway bumps/humps, and/or additional yield/stop signage are advised to mitigate for potential blocking by stacked vehicles, and pedestrian crossing. Provide a “Keep Clear” marking at the on-site drive aisle intersections so patrons who park vehicles, as well as the drive-through exit, do not get blocked if stacking extends beyond the drive-through entrance. Provide raised bumps over pedestrian cross ways.
2. Make the driveway located on the project site “Entry Only”. Exiting of the site should only be accessed via the driveway on the adjacent parcel. This would help eliminate the potential for illegal U-turns from vehicles exiting the driveway located on the project site.

Architecture

3. The building should be less vertically oriented, as horizontal emphasis is encouraged, and the use of vertical elements, such as a tower, should only be used as a focal point of the structure-such as an entryway.
4. Balance the proportion of architectural elements, such as doors and windows (and their corresponding openings), with the amount of blank wall space to help orient the building horizontally.
5. Plane breaks should be intentional and preferably functional. The elements located on, or in them, such as windows, doors, etc. should be designed in a manner that compliments those breaks.
6. The roof should be predominantly single-barrel red tile, with parapet walls used sparingly to shield any rooftop mechanical equipment. The design utilizes a disproportionate amount of parapet wall with cornice in relation to the minimal amount of red tile roof only found on tower elements.
7. Replace the cornice detail with exposed rafter tails in areas of the roof line where red tile is used.
8. The drive-through canopy should be consistent, and utilize the same architectural elements for all openings throughout its design. Ensure proportionality is maintained between the size of the columns and the visual weight of the roof.
9. Remove the banding located around the building and drive-through canopy.

10. Improve the south-facing façade as it is visible from the public right-of-way. Consider using a more SCR style door design for the employee door, and ensure the design of the electrical doors blend into the design of the building. Appurtenances on this façade should be painted to match the building color or shielded by design elements.
11. The structure's windows and doors should be a color and material that supports SCR style, such as wood or metal, painted an earth-tone color. Windows should have lites and mullions, as opposed to large single-plate windows.
12. Remove the red L.E.D. strip from the top of the building.
13. Modify the red stripe to be consistent with SCR design, such as Spanish tile.
14. Utilize an awning color consistent with SCR design, such as an earth tone red.
15. Ensure the trash enclosure is of SCR design.

Landscaping and Signage

16. Use a landscape buffer to limit direct access to the white stucco building from patrons/general public who may scuff/dirty the building.
17. Parking areas should incorporate enough landscape areas/islands so that canopy trees can be placed.
18. Place additional trees/landscaping behind the outdoor dining area to further shield the drive-through lane from Avenida Pico.
19. Ensure the trash enclosure and backflow preventer are adequately screened with landscaping.
20. All signs should be consistent with SCR design, and be made of high quality materials, such as wood, metal, glass, etc. Plastic/acrylic signs are not permitted. On the first pre-application submittal, the architectural elevations depicted a recessed arrow on one of the building facades, which is more in line with SCR.

CONCLUSION

Staff is favorable of the proposed site design as project elements are consistent with City Design Guidelines, such as the restaurant building located at the front of the lot, and automobile related functions and trash enclosure located in the rear. The placement of the outdoor dining area provides a useful transition space between Avenida Pico and the proposed building location, consistent with General Plan policies. Staff is concerned with the architecture proposed, and the building's architecture will be a major focus for improvement should a formal application be submitted. Additionally, more information is required for additional analysis of landscaping and signage. Staff seeks DRSC concurrence

with the above recommendations and any additional input on the project's design that will help ensure a project of the highest quality.

Attachments:

1. Location Map
2. Photos of existing condition
3. Visual recommendations
4. Plans

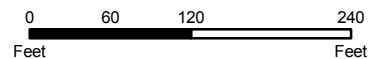


Project Site



City of San Clemente

Project: Pre-App 18-234, In-N-Out
Address: 115 Via Pico Plaza





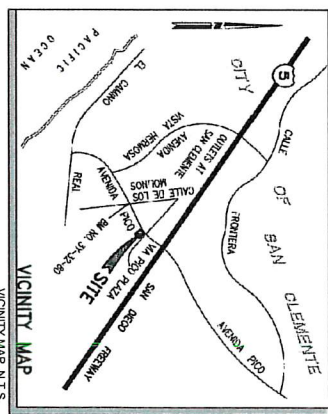
IN-N-OUT
DRIVER

900 BLESS AVENUE
 13502 MANASSAS LANE
 BALDWIN PARK, CA 91706

PRELIMINARY SITE PLAN-19

SHEET
 OF 1

SAN CLEMENTE, CA
 115 VIA PICO PLAZA



ATTACHMENT 2

SUMMARY

IN-N-OUT SITE AREA: 3.109 ACRE (67,287 SF±)

SITE ADDRESS: 115 VIA PICO PLAZA, SUITE 100

JURISDICTION: CITY OF SAN CLEMENTE, CA

APN: 692-95-1-01

LEGAL DESCRIPTION: THE LAND REFERRED TO HEREON IS SITUATED IN THE CITY OF SAN CLEMENTE, COUNTY OF ORANGE, STATE OF CALIFORNIA, PARCELS 1 AND 2, AS SHOWN ON A PARCEL MAP FILED IN BOOK 122, PAGES 36, 37, 38, AND 39 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY. SAID PARCELS 1 AND 2, ONE-SHALF OF ALL OIL, GAS AND OTHER HYDROCARBON SUBSTANCES AND MINERALS, AS RESERVED IN THE DEED FROM CLARENCE C. REED AND WIFE, TO JOY VESBERG AND WIFE, RECORDED MAY 3, 1951 IN BOOK 2183, PAGE 579 OF SAID COUNTY RECORDS, AND THE UNDERSIGNED AND WIFE, REED HIS WIFE, SINCE PASSED TO THE HEIRS, REED AND WIFE, RECORDED FEBRUARY 11, 1954 BY DEED RECORDED MARCH 1, 1954 IN BOOK 2877, PAGE 477 OF OFFICIAL RECORDS.

ZONING: CC2 - COMMUNITY COMMERCIAL, WEST PICO CORRIDOR SPECIFIC PLAN

EXISTING USE: BANK WITH DRIVE-THRU

PROPOSED USE: DRIVE-THRU FASTFOOD

EXISTING CAR STACKING IN DRIVE THRU: 20 CHAIRS MIN.

REQ. SET BUILDING SET BACKS:

FRONT (ft): 0'-0" REQ. 7'-6"

SIDE (ft): 0'-0" REQ. 6'-0"

REAR (ft): 0'-0" REQ. 178'-3"

LANDSCAPE COVERAGE: 11% REQ. 25.8% (12,318 SF)

DRIVE THRU/LANDSCAPE ALLOWED IN ZONING: YES NO

CONDITIONAL USE PERMIT NEEDED FOR DTR: YES NO

BUILDING AREA

EXISTING BANK BUILDING AREA (TWO-STORY): 6,788 S.F.

IN-N-OUT BUILDING AREA (ONE-STORY): 3,882 S.F.

BUILDING CLASSIFICATION: 100% SINGLE-SERV. FASTFOOD W/ DRIVE-THRU

CONSTRUCTION TYPE: V/B

SPRINKLERED: YES

IN-N-OUT SEATING: 87 SEATS

INDOOR: 40 SEATS

OUTDOOR: 47 SEATS

PARKING SUMMARY

IN-N-OUT BLDG AREA 3,882 SF: REQ. 15

DINING (87 SP PER 35 SF): 15

AREA 3,162 SF (75% = 15): 38

REQ SP PER 200 S.F.: 17

AREA 3,384 SF (200) = 17: 07

OUTDOOR PATIO - 40 SEATS: 8

AREA 200 SF (25) = 8: 45

EXISTING BANK PARKING: 41

****NOTE: CUMULATIVE PARKING CALCULATION OF ENTIRE PICO PLAZA TO BE COMPLETED BY LANDLORD PER CITY.**

"PRELIMINARY REVIEW" SITE PLAN LAYOUT
 (115 VIA PICO PLAZA) SAN CLEMENTE, CA

1



2



3



4

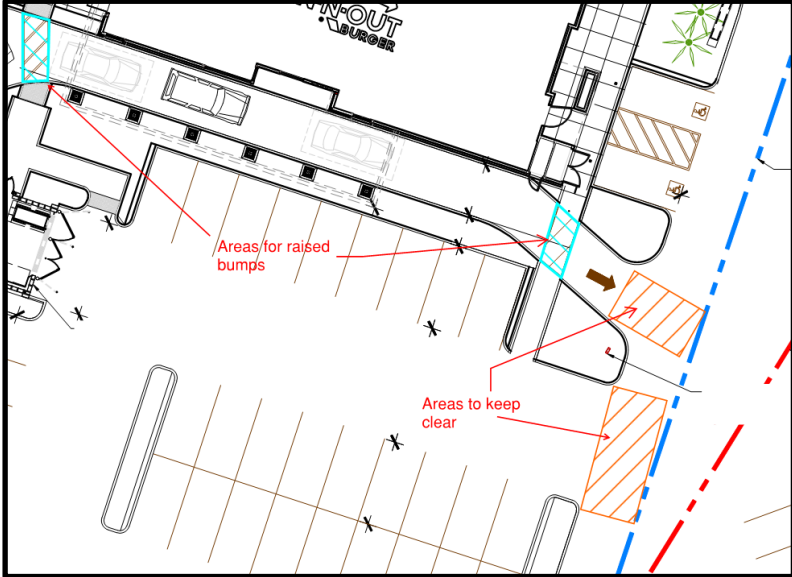
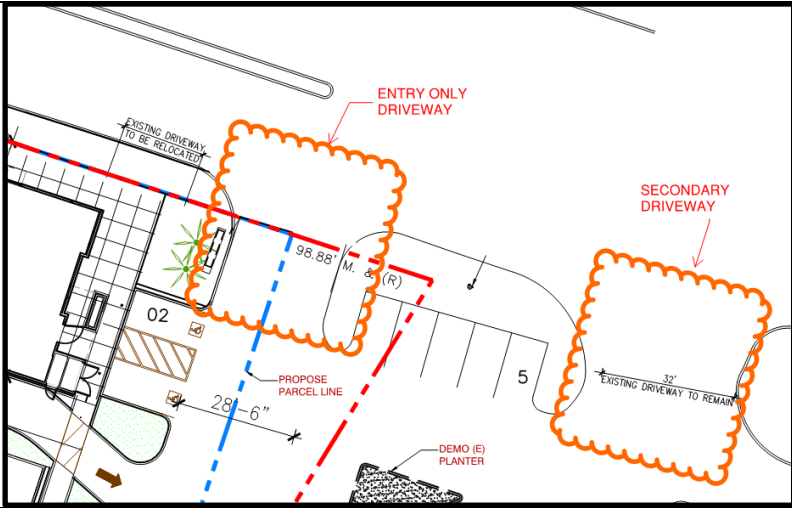








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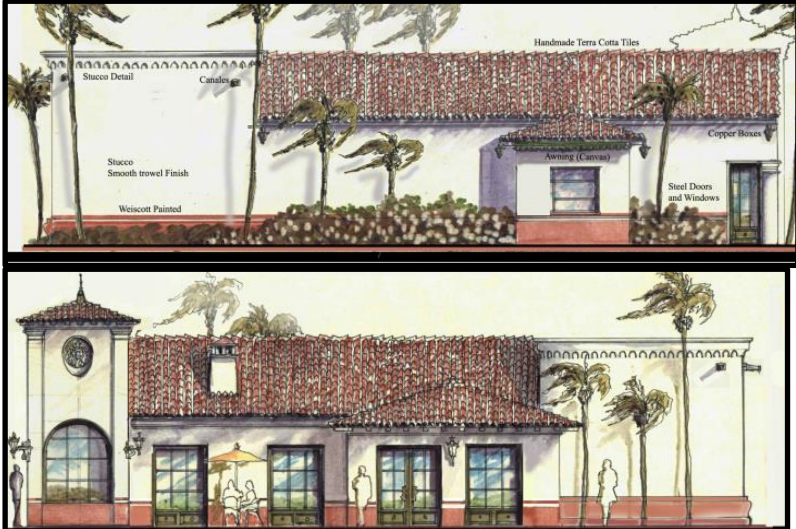
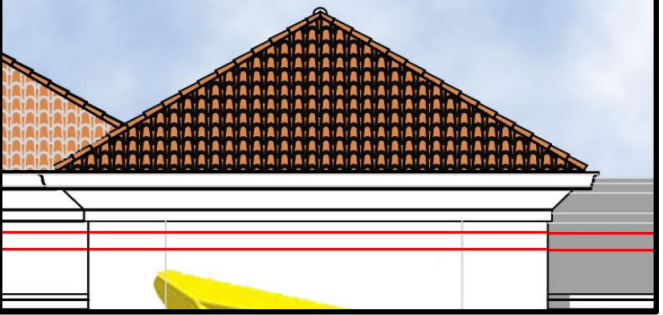
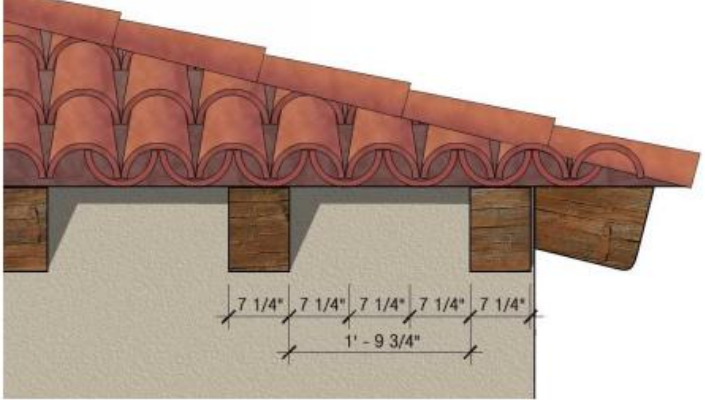
Recommendations to Improve Architectural Quality

Recommendation no. from Staff Report	Proposed	Recommendation
<p>1. The use of on-site circulation mitigation, such as keep clear signage, roadway bumps/humps, and/or additional yield/stop signage are advised to mitigate for potential blocking by stacked vehicles, and pedestrian crossing. Provide a “Keep Clear” marking at the on-site drive aisle intersections so patrons who park vehicles, as well as the drive-through exit, do not get blocked if stacking extends past the drive-through entrance. Provide raised bumps over pedestrian cross ways.</p>	<p>No circulation measures proposed.</p>	
<p>2. Make the driveway located on the project site “Entry Only”. Exiting of the site should only be accessed via the driveway on the adjacent parcel. This would help eliminate the potential for illegal U-turns from vehicles exiting the driveway located on the project site.</p>	<p>No limitation on entry/exit proposed.</p>	

Recommendations to Improve Architectural Quality

Recommendation no. from Staff Report	Proposed	Recommendation
<p>3. The building should be less vertically oriented, a horizontal emphasis is encouraged, and the use of vertical elements such as a tower should only be used as a focal point of the structure, such as an entryway.</p> <p>4. Balance the proportion of architectural elements such as doors and windows (and their corresponding openings) with amount of blank wall space to help orient the building horizontally.</p>		
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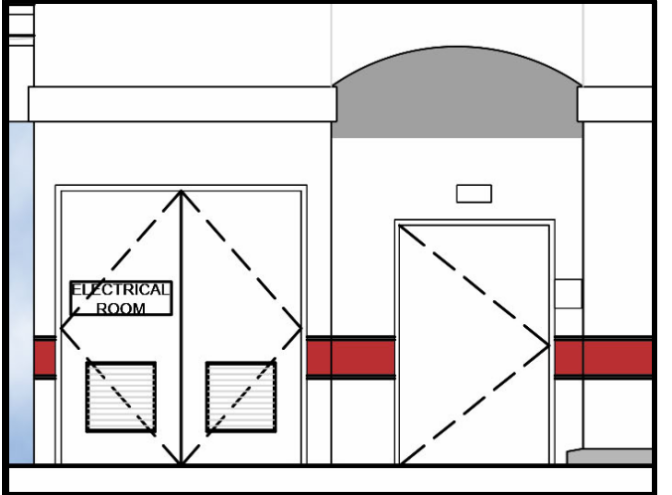
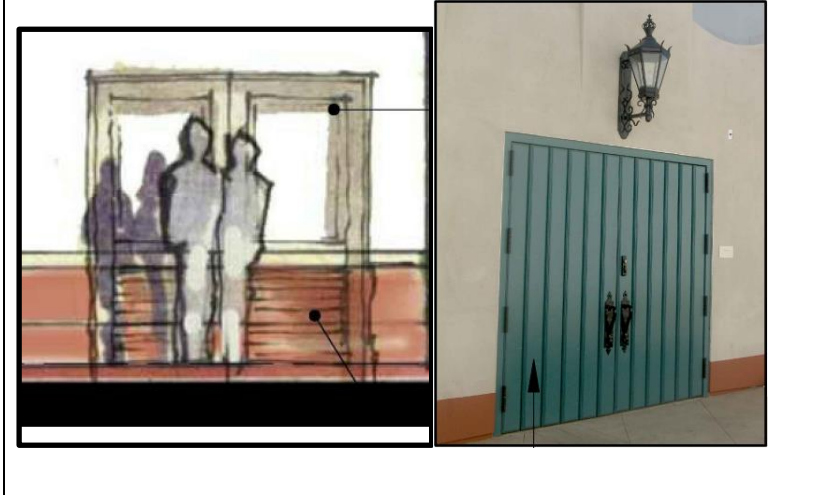


Recommendations to Improve Architectural Quality

Recommendation no. from Staff Report	Proposed	Recommendation
<p>6. The roof should be predominantly single-barrel red tile, with parapet walls used sparingly to shield any rooftop mechanical equipment. The design utilizes a disproportionate amount of parapet wall with cornice in relation to the minimal amount of red tile roof only found on tower elements.</p>		
<p>7. Replace the cornice detail with exposed rater tails in areas of the roof line where red tile is used.</p>		

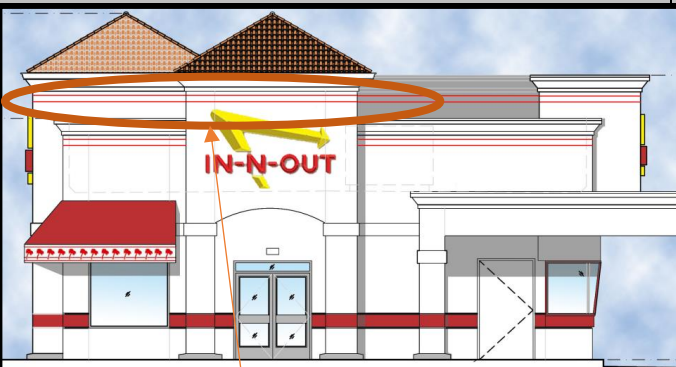
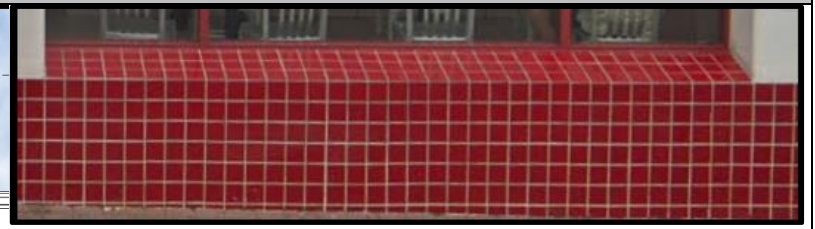


Recommendations to Improve Architectural Quality

Recommendation no. from Staff Report	Proposed	Recommendation
<p>8. The drive-through canopy should be consistent, and utilize the same architectural elements for all openings throughout its design. Ensure proportionality is maintained between the size of the columns and the visual weight of the roof.</p>		
<p>9. Remove the banding located around the building and drive-through canopy.</p>	<p>—</p>	


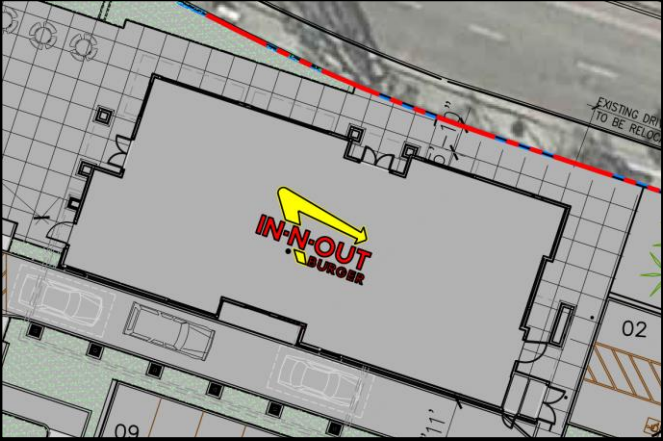
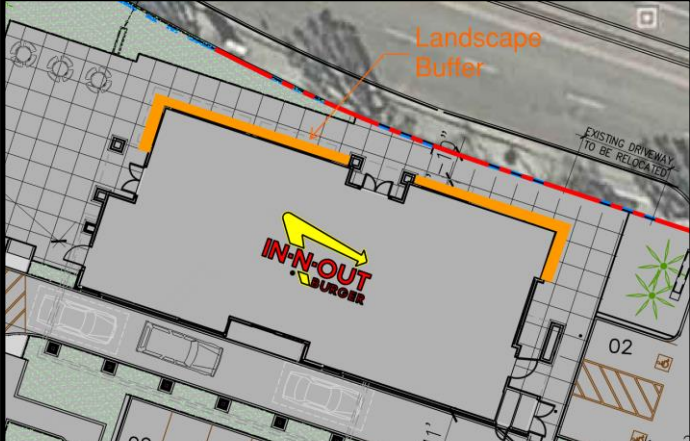
Recommendations to Improve Architectural Quality

Recommendation no. from Staff Report	Proposed	Recommendation
<p>10. Improve the south-facing façade as it is visible from the public right-of-way. Consider using a more SCR style door design for the employee door, and ensure the design of the electrical doors blend into the design of the building. Appurtenances on this façade should be painted to match the building color or shielded by design elements.</p>	 <p>An architectural elevation drawing of a building facade. It shows a central door with a transom window above it. To the left of the door is a section labeled 'ELECTRICAL ROOM' with two windows. The drawing uses dashed lines to indicate the placement of the door and windows. A red horizontal band is shown below the door and windows.</p>	 <p>Two photographs. The left one is a sketch of a door with a transom window, showing silhouettes of people walking through. The right one is a photograph of a teal-colored door with a decorative light fixture mounted above it.</p>
<p>11. The structure's windows and doors should be a color and material that supports SCR style, such as wood or metal, painted an earth-tone color. Windows should have lites and mullions, as opposed to large single-plate windows.</p>	 <p>Two architectural elevation drawings of an IN-N-OUT building facade. The top drawing shows a full view of the building with a red awning and a yellow lightning bolt logo. The bottom drawing shows a similar view but with a dashed line indicating a proposed change to the window design.</p>	 <p>Three photographs. The top left shows a large arched window with a green mesh screen. The top right shows a dark-framed door with a transom window and the address number 1023. The bottom right shows a glass entrance door with a FedEx logo.</p>

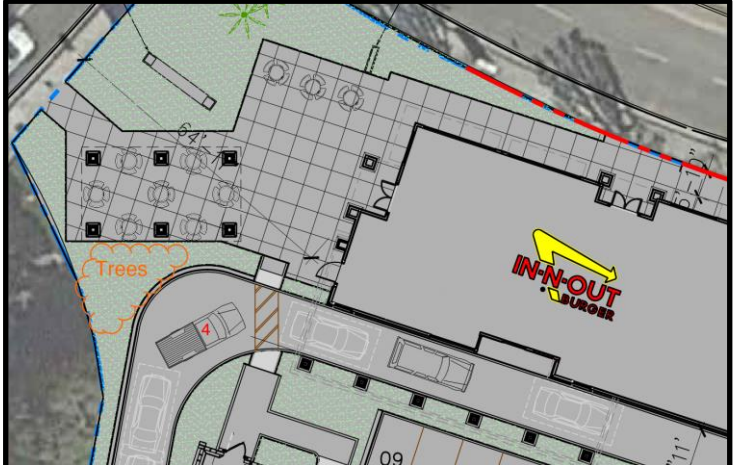
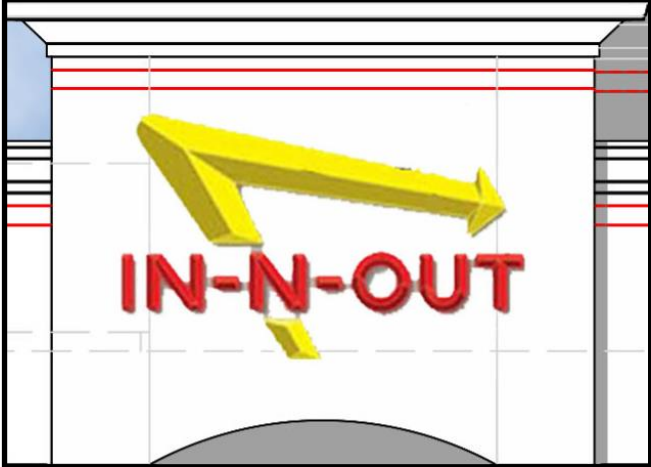
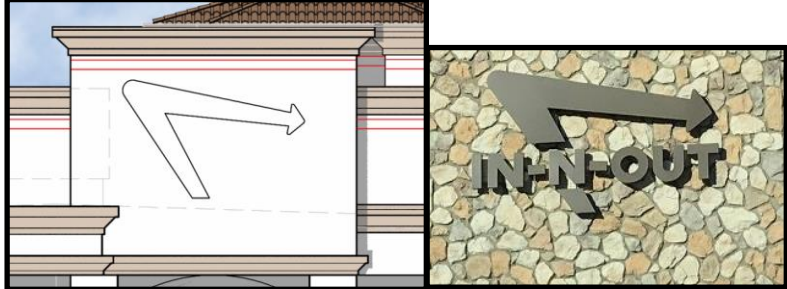
Recommendations to Improve Architectural Quality

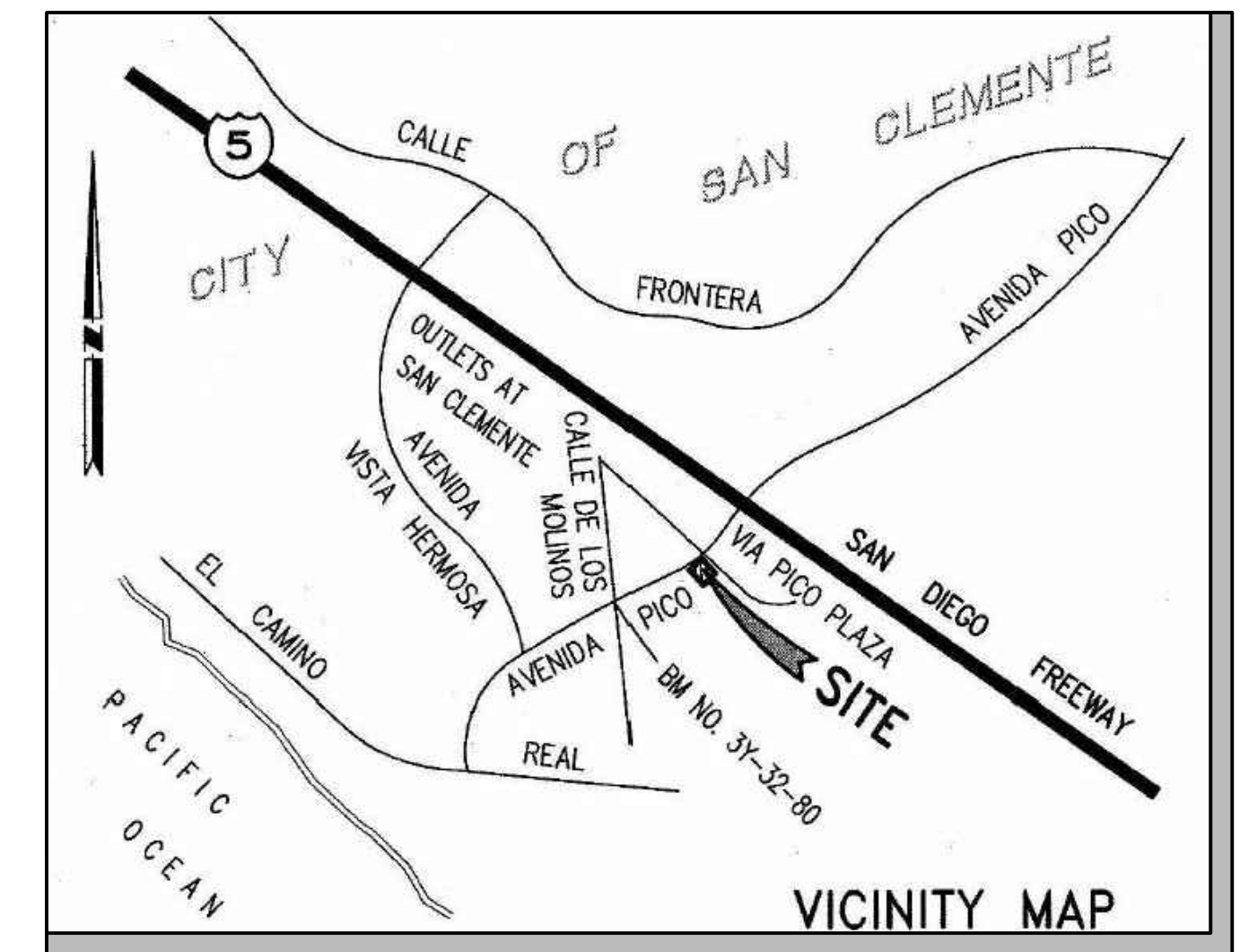
Recommendation no. from Staff Report	Proposed	Recommendation
<p>12. Remove the red L.E.D. strip from the top of the building.</p> <p>13. Modify the red stripe to be consistent with SCR design, such as Spanish tile.</p>	 <p data-bbox="714 706 1092 747">Remove all L.E.D. lighting</p>	
<p>14. Utilize an awning color consistent with SCR design, such as an earth tone red.</p>		

Recommendations to Improve Architectural Quality

Recommendation no. from Staff Report	Proposed	Recommendation
<p>5. Ensure the trash enclosure is of SCR design.</p>	<p>No elevations proposed.</p>	 <p>The top image is an architectural elevation drawing of a trash enclosure. It features a pergola-style roof with dark wooden beams and a central window with vertical metal bars. The walls are light-colored with a darker base. Callouts '4' and '12' are present, along with a '1ST F.F. +0'' marker. The bottom image is a photograph of the existing trash enclosure, which is a dark-colored metal structure with a white pillar and a pergola roof, situated outdoors near a white wall and trees.</p>
<p>16. Use a landscape buffer to limit direct access to the white stucco building from patrons/general public who may scuff/dirty the building.</p>	 <p>The diagram shows a site plan with a grey building footprint labeled 'IN-N-OUT BURGER'. A red dashed line indicates the 'EXISTING DRIVEWAY TO BE RELOCATED'. The building is adjacent to a parking area with spaces labeled '02' and '09'.</p>	 <p>This diagram is similar to the previous one but includes an orange shaded area around the building labeled 'Landscape Buffer'. The 'EXISTING DRIVEWAY TO BE RELOCATED' is also indicated.</p>

Recommendations to Improve Architectural Quality

Recommendation no. from Staff Report	Proposed	Recommendation
<p>18. Place additional trees/landscaping behind the outdoor dining area to further shield the drive-through lane from Avenida Pico.</p>	<p>None depicted in this location.</p>	
<p>20. All signs should be consistent with SCR design, and be made of high quality materials, such as wood, metal, glass, etc. Plastic/acrylic signs are not permitted. On the first pre-application submittal, the architectural elevations depicted a recessed arrow on one of the building facades, which is more inline with SCR.</p>		



VICINITY MAP N.T.S.

SUMMARY

IN-N-OUT SITE AREA: ± 1.09 ACRE (47,787 SF ±)
SITE ADDRESS: 115 VIA PICO PLAZA SUITE 100
JURISDICTION: CITY OF SAN CLEMENTE, CA

LEGAL DESCRIPTION: THE LAND REFERRED TO HEREON IS SITUATED IN THE CITY OF SAN CLEMENTE, COUNTY OF ORANGE, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:
 PARCEL 1 AND 2, AS SHOWN ON A PARCEL MAP FILED IN BOOK 122, PAGES 36, 37, 38, AND 39 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.
 EXCEPT FROM A PORTION OF SAID PARCELS 1 AND 2, ONE-HALF OF ALL OIL, GAS AND OTHER HYDROCARBON SUBSTANCES AND MINERALS, AS RESERVED IN THE DEED FROM CLARENCE C. REED AND WIFE, TO ROY VISBEEK AND WIFE, RECORDED MAY 3, 1951 IN BOOK 2183, PAGE 579 OF OFFICIAL RECORDS. THE INTEREST OF CLARENCE C. REED AND WIFE HAS SINCE PASSED TO CLARENCE C. REED AND MARGARET U. REED HIS WIFE, BY DEED RECORDED MARCH 1, 1954 IN BOOK 2677, PAGE 477 OF OFFICIAL RECORDS.

ZONING: CC2 - COMMUNITY COMMERCIAL, WEST PICO CORRIDOR SPECIFIC PLAN

EXISTING USE: BANK WITH DRIVE-THRU
PROPOSED USE: DRIVE-THRU FASTFOOD
CAR STACKING IN DRIVE THRU: 20 CARS MIN.

REQ. SET BUILDING SET BACKS	REQ.	PROV'D
FRONT (N):	0'-0"	7'-6"
SIDE (W):	0'-0"	64'-11"
SIDE (E):	0'-0"	60'-2"
REAR (S):	0'-0"	178'-3"
LANDSCAPE COVERAGE:	11%	25.8% (12,318 S.F.)
DRIVE THRUS ALLOWED IN ZONING	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>
CONDITIONAL USE PERMIT NEED FOR D/T:	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>

BUILDING AREA

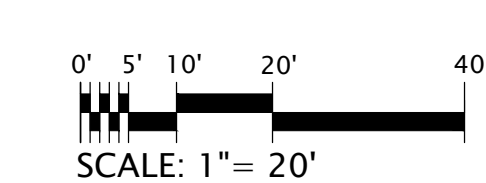
EXISTING BANK BUILDING AREA (TWO- STORY): 6,708 S.F.
IN-N-OUT BUILDING AREA (ONE- STORY): 3,882 S.F.
BUILDING CLASSIFICATION: A-2
USE: 100% SINGLE-SERV. FASTFOOD W/ DRIVE-THRU
CONSTRUCTION TYPE: V-B
SPRINKLERED: YES
IN-N-OUT SEATING:
 INDOOR: 97 SEATS
 OUTDOOR: 40 SEATS

PARKING SUMMARY

	REQ.	PROV'D
IN-N-OUT BLDG AREA 3,882 SF :		
DINING (@1 SP PER 35 S.F.)		
AREA 518 SF / 35 = 15	15	38
BLDG - AREA NON-DINING (@1 SP PER 200 S.F.)		
AREA 3364 SF / 200 = 17	17	07
OUTDOOR PATIO: 40 SEATS		
AREA 200 SF/35 = 6	6	
	38	45
EXISTING BANK PARKING:	41	

**NOTE: CUMULATIVE PARKING CALCULATION OF ENTIRE PICO PLAZA TO BE COMPLETED BY LANDLORD PER CITY.

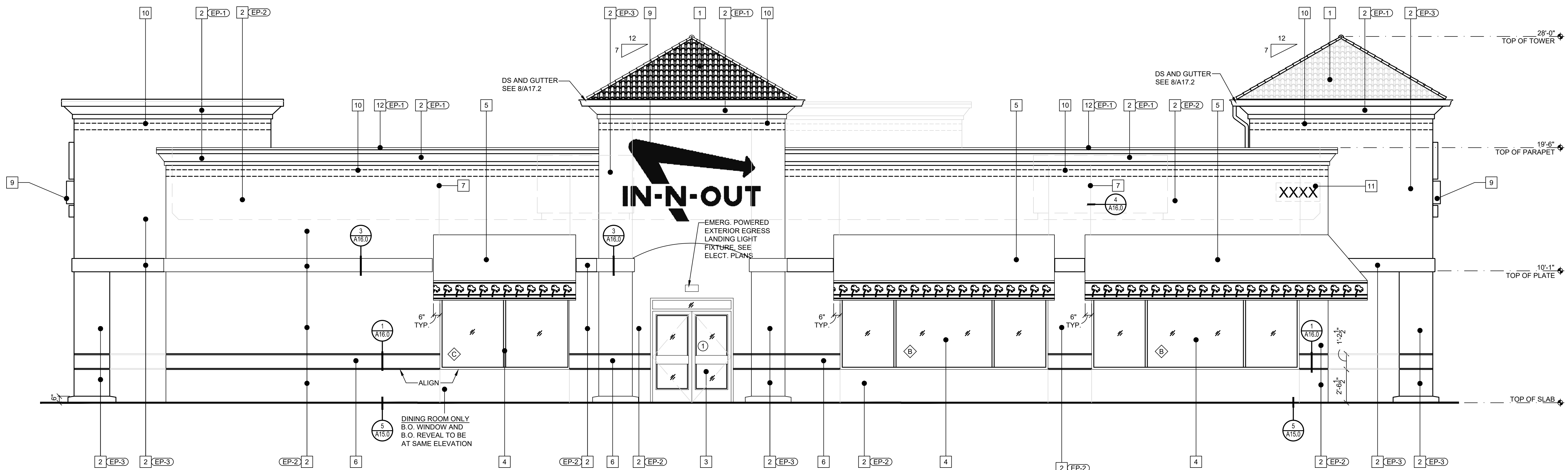
DATE	REVISIONS
3-28-18	PLANNING SUBMITTAL / REVISIONS
06-06-18	PLANNING SUBMITTAL / REVISIONS



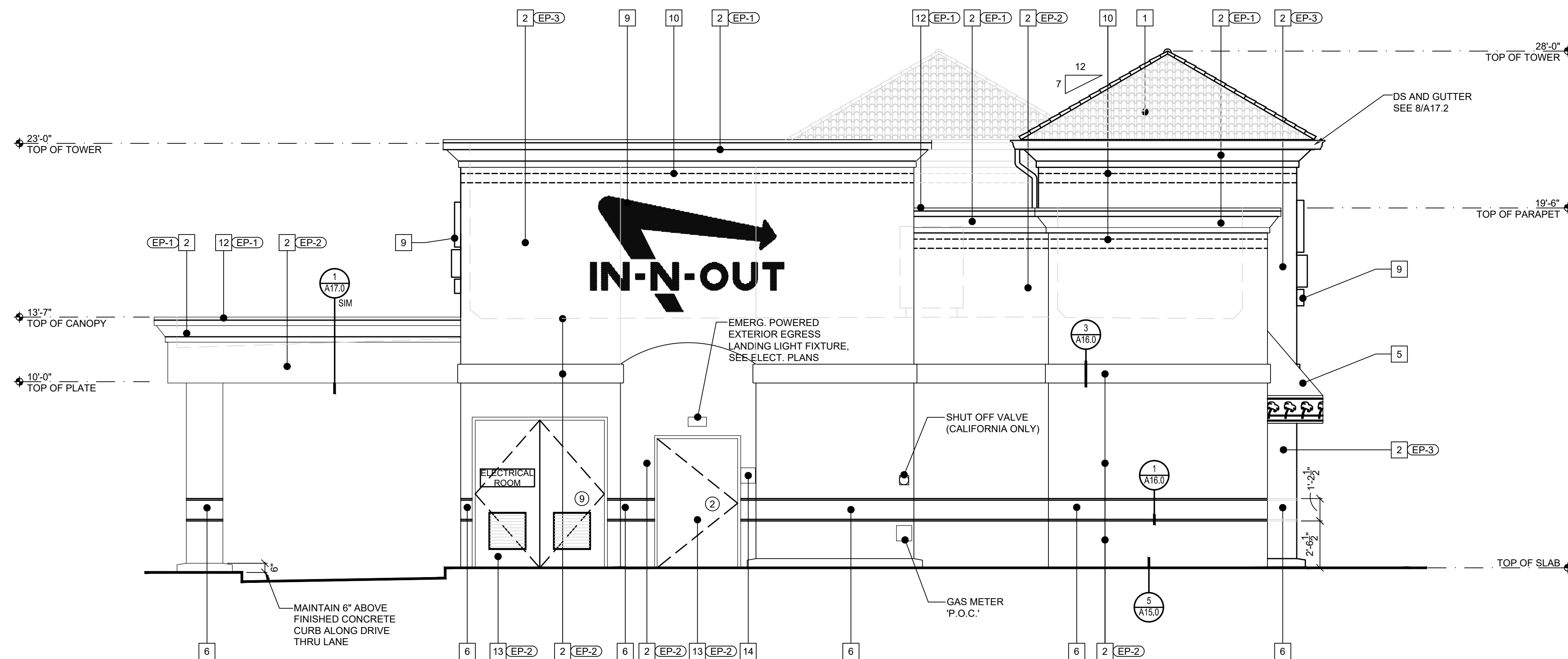
PRELIMINARY SITE PLAN-19

SAN CLEMENTE, CA
 115 VIA PICO PLAZA

"PRELIMINARY REVIEW" SITE PLAN LAYOUT
 (115 VIA PICO PLAZA) SAN CLEMENTE, CA



1 NORTH ELEVATION
SCALE: 1/4"=1'-0"



2 EAST ELEVATION
SCALE: 1/4"=1'-0"

KEY NOTES

1	TILE ROOF - EAGLE ROOFING PRODUCTS, CAPISTRANO STYLE 3125 TERRACOTTA. PROVIDE BIRD STOP AND PAINT TO MATCH TILE.
2	STUCCO - MEDIUM SAND FLOAT TEXTURE FINISH.
3	ALUMINUM STOREFRONT DOORS - CLEAR ANODIZED ALUMINUM.
4	ALUMINUM WINDOWS - CLEAR ANODIZED ALUMINUM.
5	AWNINGS - PVC COATED WICK-RESISTANT ERADICABLE POLYESTER AWNINGS WITH CUSTOM PALM TREE ERADICATED TO WHITE COLOR BY COOLEY BRITE CUSTOM RED 79-L1124A. FLAME RETARDANT PER STATE FIRE MARSHALL UL-48, UL-94, NFPA 701. PROVIDE FLUORESCENT BACKLIGHTING. SEE: 5 & 6 A/16.0.
6	STUCCO BAND PAINT (EP-4). INTEGRAL STUCCO COLOR TO MATCH PAINT (EP-4). 1" ALUMINUM REVEAL TOP AND BOTTOM (DO NOT PAINT REVEALS). SEE 1/A16.0 & 13/A17.0
7	1/4" WIDE METAL PENN SCREED. SEE DETAIL 4/A16.0.
8	SLOPED ALUMINUM WINDOWS - U.S. ALUMINUM S-010. GLASS JOINTS TO HAVE POLISHED EDGES WITH SILICONE JOINTS. PAINT WOOD FRAME IN-N-OUT RED.
9	IN-N-OUT BURGER ILLUMINATED LOGO SIGN UNDER SEPARATE PERMIT.
10	L.E.D. DOUBLE BAND LIGHTING - UNDER SEPARATE PERMIT.
11	BUILDING ADDRESS NUMBERS TO BE 12" HIGH, 4" MIN. W/ MIN. STROKE WIDTH OF 0.5 INCH PER FIRE DEPARTMENT AND CITY JURISDICTION ADDRESS DISPLAY REQUIREMENTS: 1. ADDRESS SHALL BE DISPLAYED AND VISIBLE FROM BOTH STREET DIRECTIONS OF APPROACHING VEHICLES. 2. PERMANENT NUMBERS AND LETTERS SHALL BE MADE OF DURABLE AND CLEARLY VISIBLE MATERIAL SUCH AS WOOD, METAL, CERAMIC, PLASTIC AND VINYL. (PAINTED OR GLUED ON NUMBERS ARE NOT ACCEPTABLE MATERIALS). 3. NUMBERS SHALL BE OF COLORS CONTRASTING WITH THE BACKGROUND TO WHICH THEY ARE ATTACHED. 4. NUMBERS SHALL BE A MINIMUM PROPORTION RATIO OF 6:1 (HEIGHT TO WIDTH). 5. NUMBERS CANNOT BE SPELLED.
12	METAL CAP-COLOR TO MATCH STUCCO.
13	HOLLOW METAL DOOR - SEE SHEET A11.0
14	5'-0 HIGH STUB OUT FOR C62 LINE
15	4" MIN. HIGH WHITE LETTERING "RISER ROOM" ON RED BACKGROUND SIGN. MOUNT ON RISER ROOM DOOR
16	RECESSED KNOX BOX AT 5'-0" HIGH TO THE RIGHT OF FIRE RISER ROOM. SEE DETAIL 1/A17.1

EXTERIOR PAINT SCHEDULE

NO.	MANUFACTURER - COLOR #	COLOR NAME	REMARKS
EP-1	DUNN EDWARDS COLOR - DEW 339	BONE CHINA	
EP-2	DUNN EDWARDS COLOR - DEW 339	BONE CHINA	
EP-3	DUNN EDWARDS COLOR - DEW 339	BONE CHINA	
EP-4	DUNN EDWARDS COLOR - DEA 107	HOT JAZZ	

DATE	REVISIONS
3-28-18	PLANNING SUBMITTAL / REVISIONS
11-27-18	PLANNING SUBMITTAL / REVERSE BUILDING



13502 HAMBURGER LANE
BALDWIN PARK, CA 91706

ELEVATION-1

SHEET OF 1

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SAN CLEMENTE, CA

115 VIA PICO PLAZA

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"PRELIMINARY REVIEW" ELEVATION LAYOUT
(115 VIA PICO PLAZA) SAN CLEMENTE, CA



13502 HAMBURGER LANE
BALDWIN PARK, CA 91706

ELEVATION-2

SHEET OF 1

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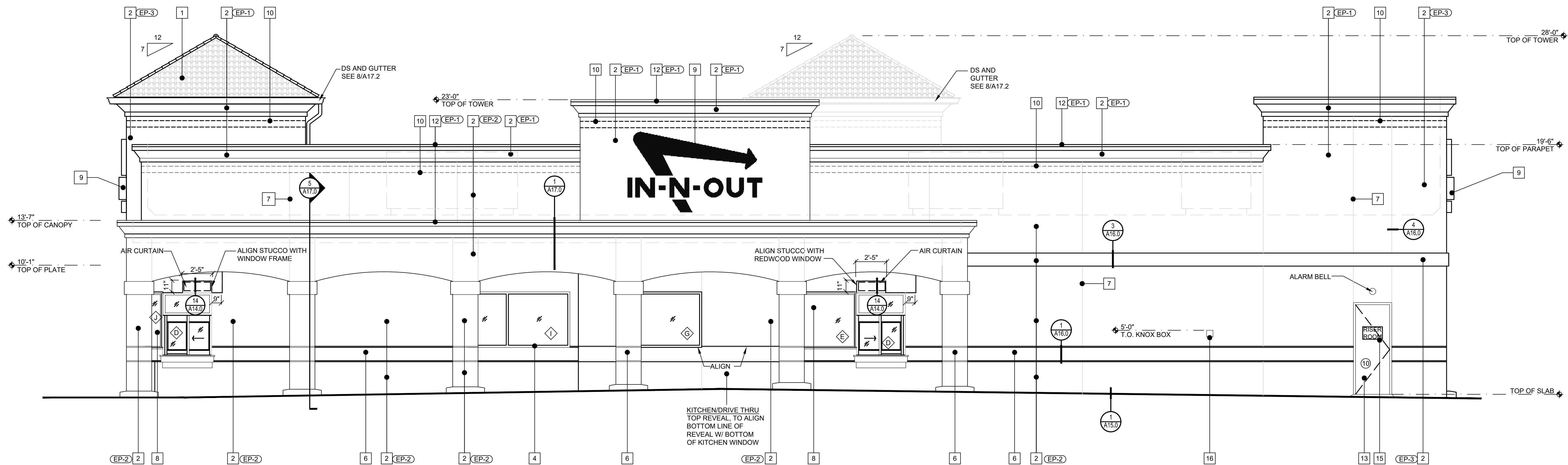
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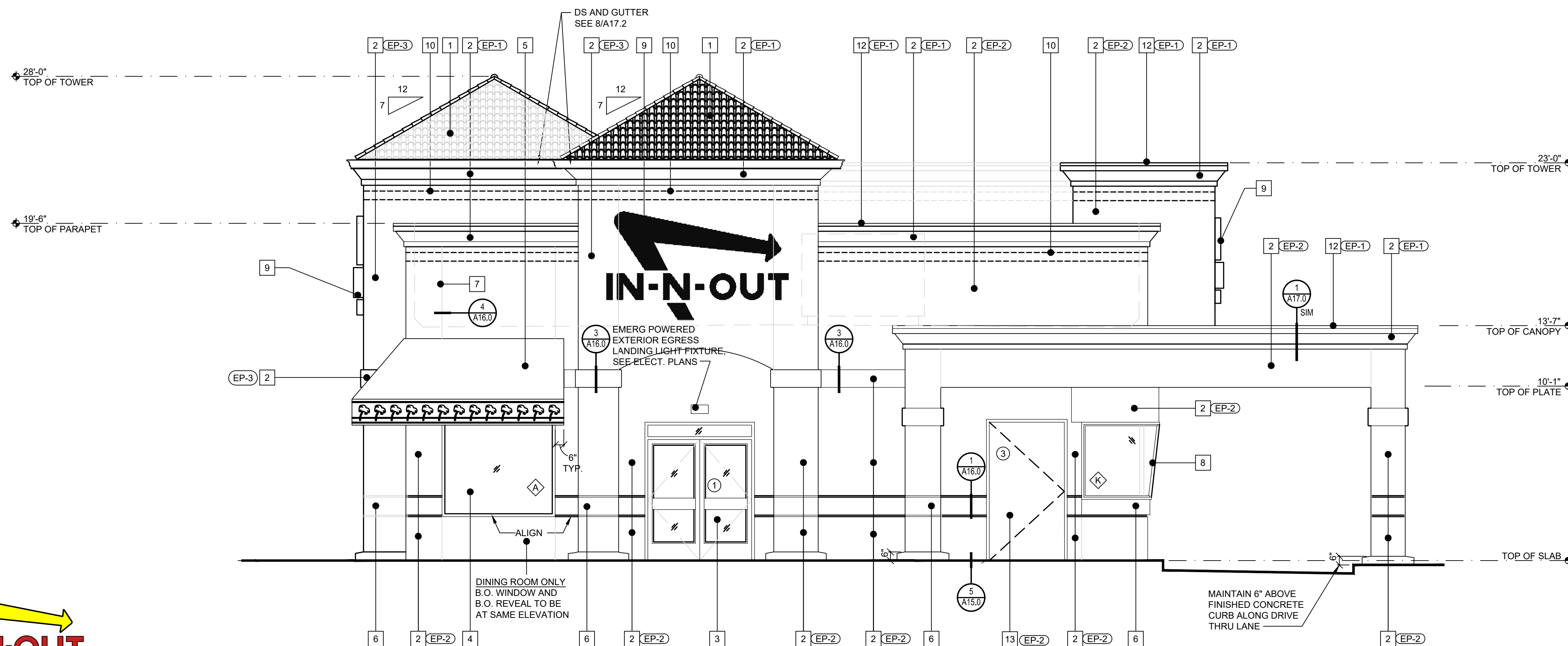
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11-27-18

"PRELIMINARY REVIEW" ELEVATION LAYOUT
(115 VIA PICO PLAZA) SAN CLEMENTE, CA



1 SOUTH ELEVATION
SCALE: 1/4"=1'-0"



2 WEST ELEVATION
SCALE: 1/4"=1'-0"

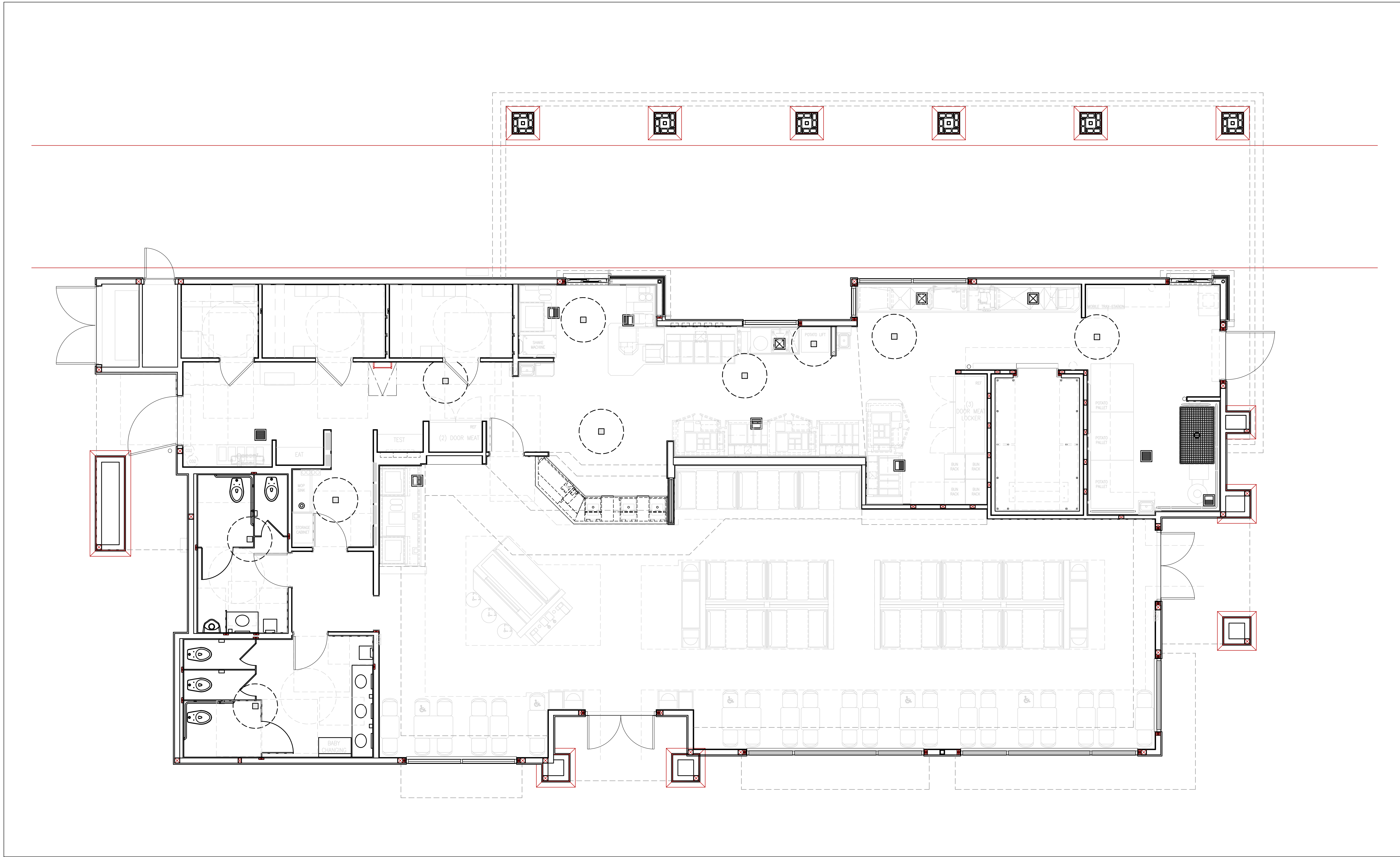
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DATE	REVISIONS
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11-27-18	PLANNING SUBMITTAL / REVERSE BUILDING



"PRELIMINARY REVIEW" FLOOR PLAN LAYOUT
 (115 VIA PICO PLAZA) SAN CLEMENTE, CA



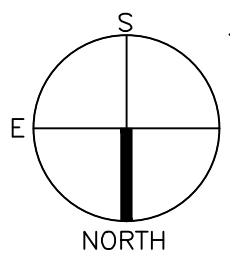
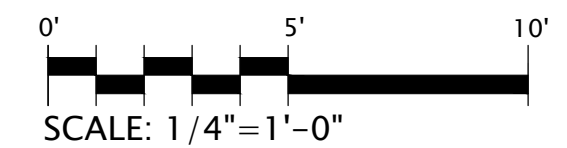
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FLOOR PLAN

SHEET OF 1

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115 VIA PICO PLAZA



DATE	REVISIONS
3-28-18	PLANNING SUBMITTAL / REVISIONS
11-27-18	PLANNING SUBMITTAL / REVERSE BUILDING

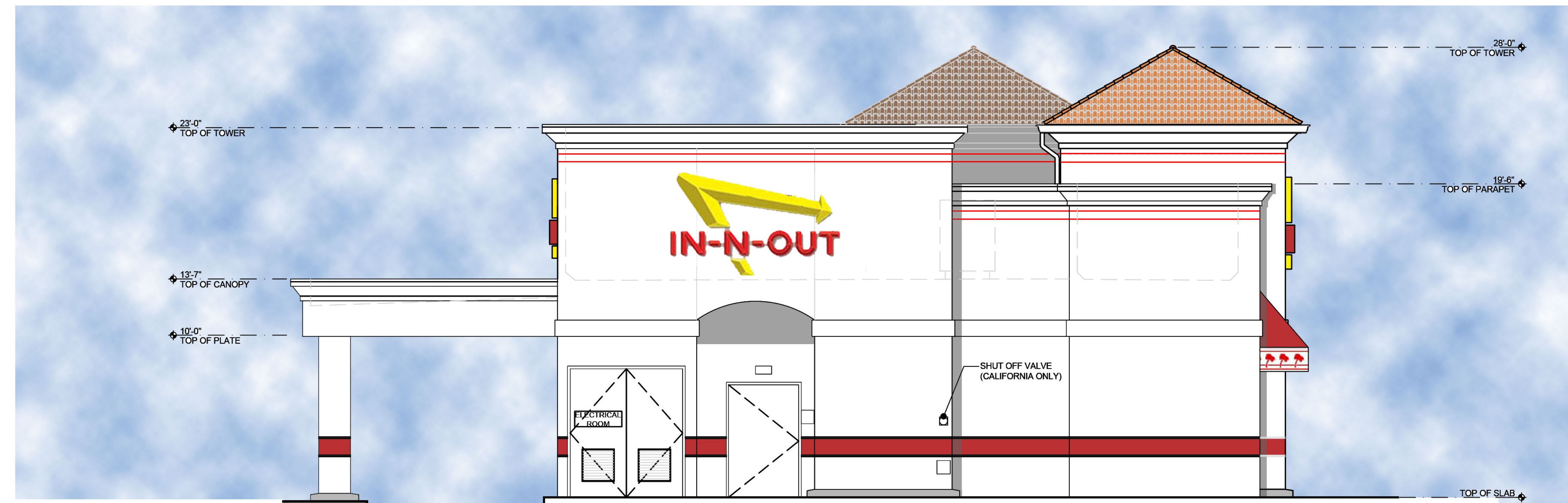
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11-27-18





1 NORTH ELEVATION
A8.0 SCALE: 1/4"=1'-0"



2 EAST ELEVATION
A8.0 SCALE: 1/4"=1'-0"



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ELEVATION-1

SHEET OF 1

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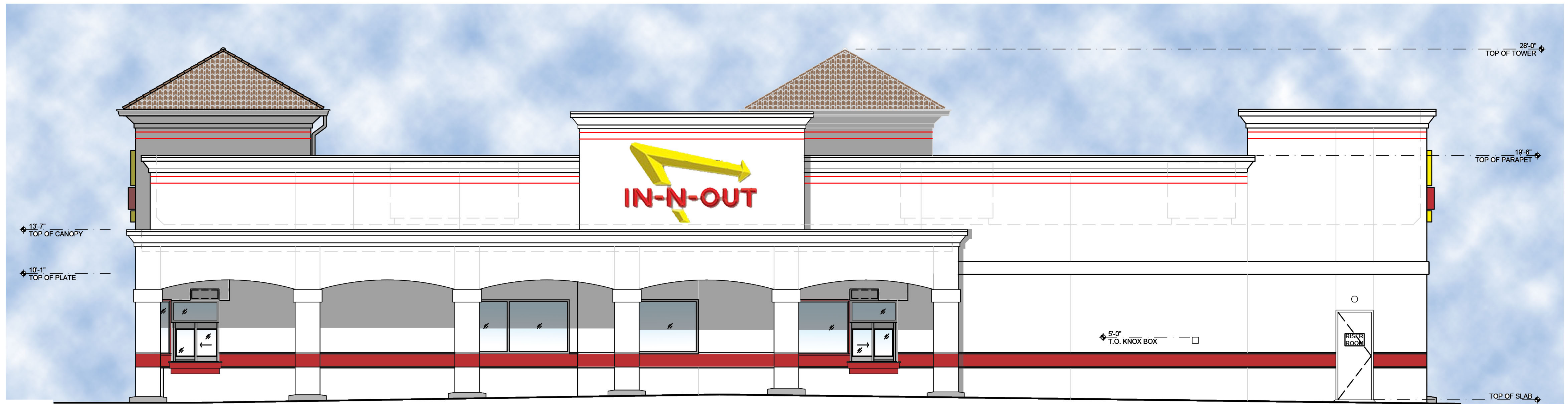
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DATE	REVISIONS
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11-27-18	PLANNING SUBMITTAL / REVISE BUILDING

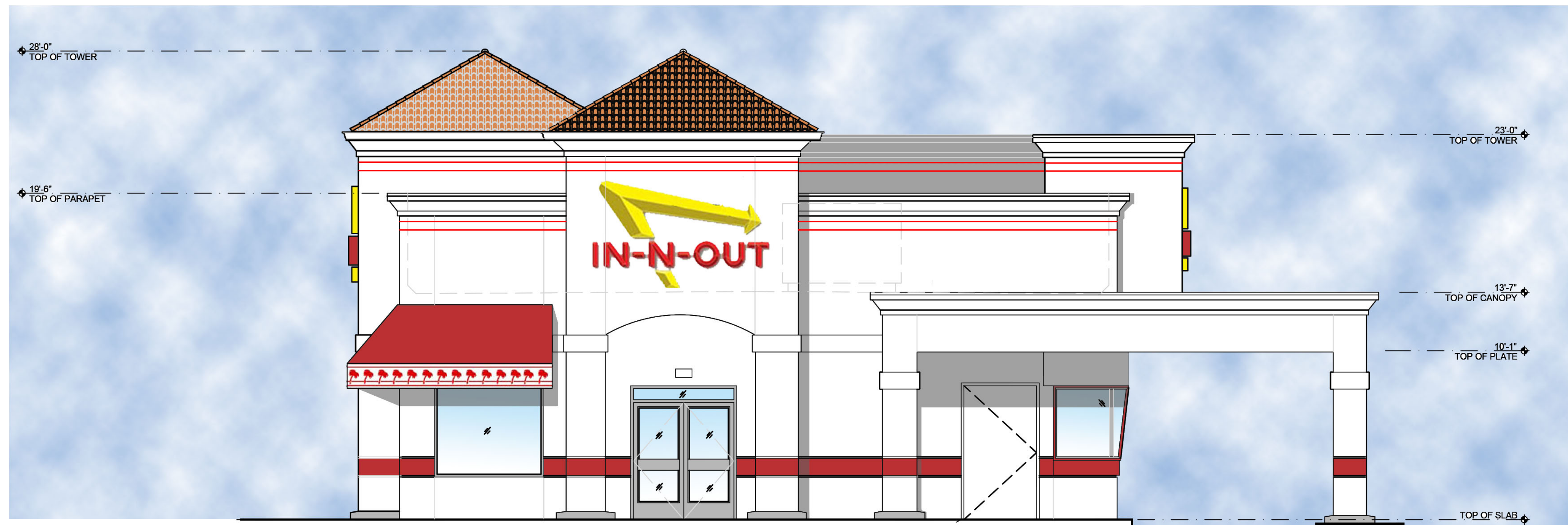
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11-27-18

"PRELIMINARY REVIEW" ELEVATION LAYOUT
(115 VIA PICO PLAZA) SAN CLEMENTE, CA



1 SOUTH ELEVATION
A9.0 SCALE: 1/4"=1'-0"



2 WEST ELEVATION
A9.0 SCALE: 1/4"=1'-0"



13502 HAMBURGER LANE
BALDWIN PARK, CA 91706

ELEVATION-2

SHEET OF 1

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115 VIA PICO PLAZA

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"PRELIMINARY REVIEW" ELEVATION LAYOUT
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11-27-18